

Soch Sails DF65 Rig Tuning Guide

A simple yet useful guide to tuning your rigs for a DF65 written in a manner that you can understand without needing a degree in rocket science or membership of MENSA.



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Soch Sails DF65 Rig Tuning Manual

Thanks for taking the time to read this short guide to tuning your rigs for your DF65. None of this is complicated stuff but it will allow you to put on a good base tune which works. You can then experiment to suit your sailing style, just remember to make a note of your measurements when you are happy.

Important Information.

We've been making sails for the DF65 now for a while now and have raced them at local, national and international events, we've never been upset with our sails and more importantly neither have any of our customers. The tuning information provided in this short guide is applicable to all DF65's. Your mates mate might do it differently and you're more than welcome to follow his advice, what you are about to read are instructions which come from two and a half years of racing DF65's to this recipe and the rigs we race are base tuned exactly to this base spec.

OK, so let's begin.

Introduction

As we've mentioned above this might not be everyone's cup of tea when it comes to tuning their DF65 however there are a few assumptions that we have made. You'll need to make sure that you have done the following things to at least give a good base for setting your tune. We've listed these below.

1. Read, understood and actioned the hints and tips from the DF65 International website or the UK NCA website.
2. Waterproofed your boat, specifically the deck eyes. I know it's in the H&T but it's worth another mention!
3. Built your rig correctly, sounds easy but the number of poorly and incorrect rigs we see is scary! See the Soch Sails rig building guide if you need help.
4. Made sure that your jib boom is close to the foredeck, as per the drawing in the rig building guide, no matter how hard you try if your jib boom is 50mm off the foredeck you'll struggle to get any sort of tune on your rig. See the rig building guide again, it's all in there, honest.

Items to get in front of you before starting.

Cup of tea – most important.

Snacks – flapjack or cookies are good.

Tape measure – either a builders tape or a soft dressmakers tape are good here.

Plastic Ruler – 300mm is plenty

Step 1.

Get your boat out and set the mast gate first. The measurement we have used here is from the back of deck eye 2 (the one that the jib tiedown goes through) to the front of the mast step, (the round hole that the mast slots in to). Set it to the position for the rig you are going to put in.

Step 2.

Pop your chosen rig in and rig it as normal, pop the keel in and sheet the rig in.

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Step 3.

Set up the fore and aft rake using the dimensions given on the pictures below. The first time you do this it is best to loosen off the jib uphaul and forestay bowsies. The fore measurement is taken from the back edge of the bow bumper, where it meets the hull, to the bottom of the uppermost metal mast ring. The aft measurement is taken from the top of the backstay hook to the bottom of metal mast ring above the jib hanger. These measurements work for all but V1 boats which have a smaller bow bumper. If you have one of these boats then take the fore measurement from a point 105mm forward of deck eye 2 (the one the jib swivel goes through). Once achieved you can tweak up the bowsies on the jib and forestay so they are just tight.

Step 4.

Using the bowsies on the booms set up the distance from centreline of the boat to the boom, 40-45mm on the Jib boom and 10-20mm on the mainboom. These distances are the same for the three rigs.

Step 5.

Having completed the above step set up the draft of the sails, these should be as follows, 20-25mm for the jib and 20mm for the main.

Step 6.

Now lay the boat on its side so you can sight down the mast. Using the middle of the three main leach battens set the vang up so that the vertical distance between the leach of the sail and the backstay is correct for the rig you have on the boat.

Step 7.

Do the same for the jib using the bottom jib batten on the leach to the jib topping lift as your vertical distance.

Step 8.

Tweak the backstay so that the curve of the mast matches the luff curve on the main sail.

Step 9.

Go to the lake and go sailing.....do it now...go!!

What next?

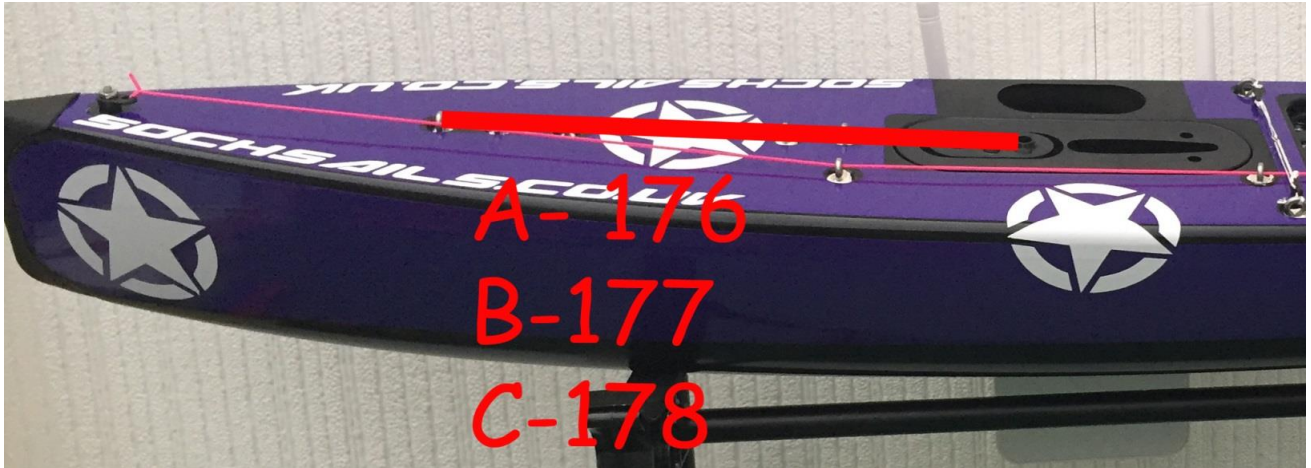
This should give you a good base setup for a correctly built rig. You can then begin to experiment with the setup of your boat to suit your style of sailing. Remember to keep notes of what works as a fast setting for you and what the conditions were, where you were racing etc etc.

This will allow you to build a good record of your fast settings and more importantly it will allow you to repeat those settings time and time again which then gives you more time to look around the racecourse at where you are and where everyone else is and what they are up to!!

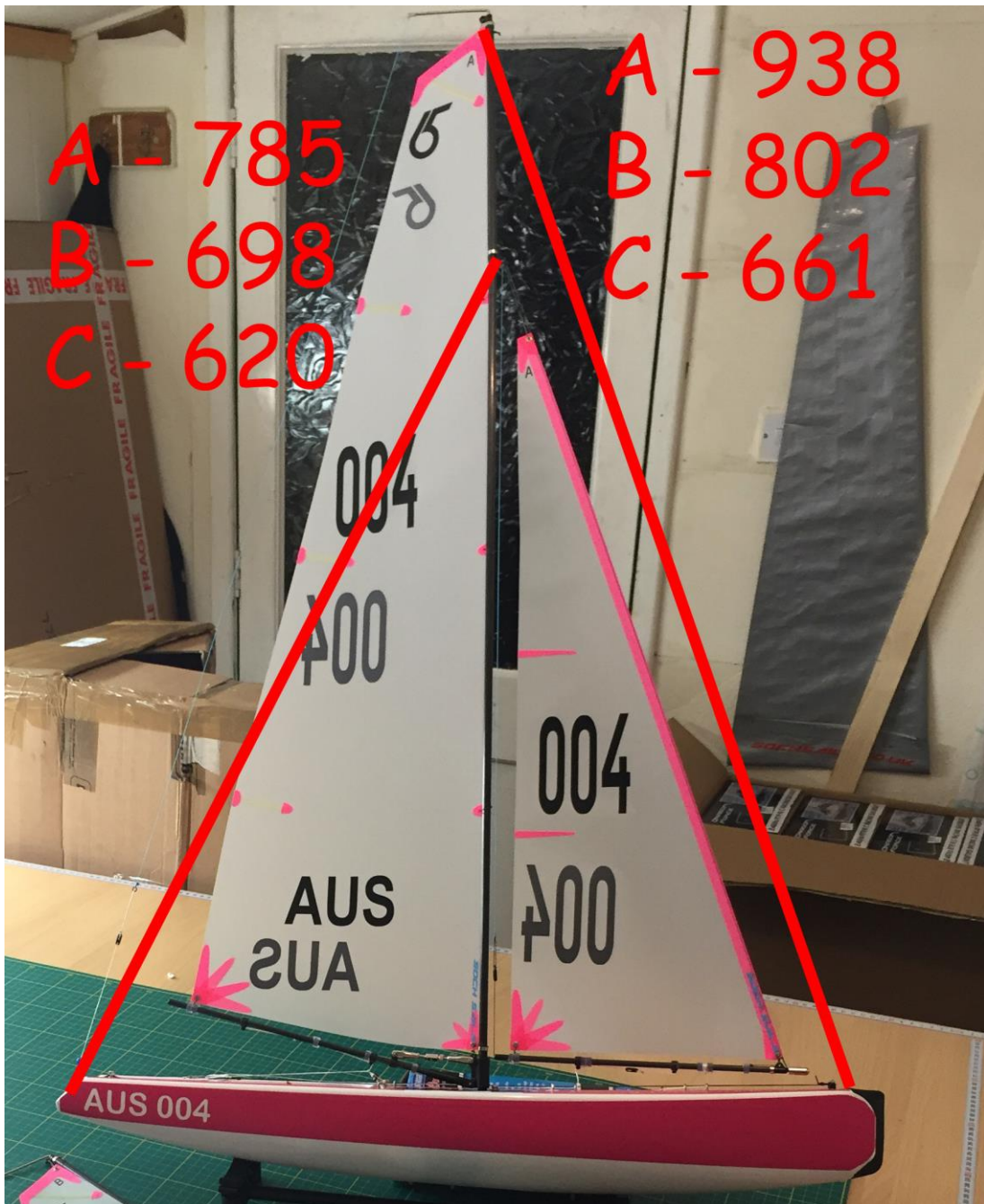
We hope that this has been a useful guide for you, if you have any comments then please let us know.

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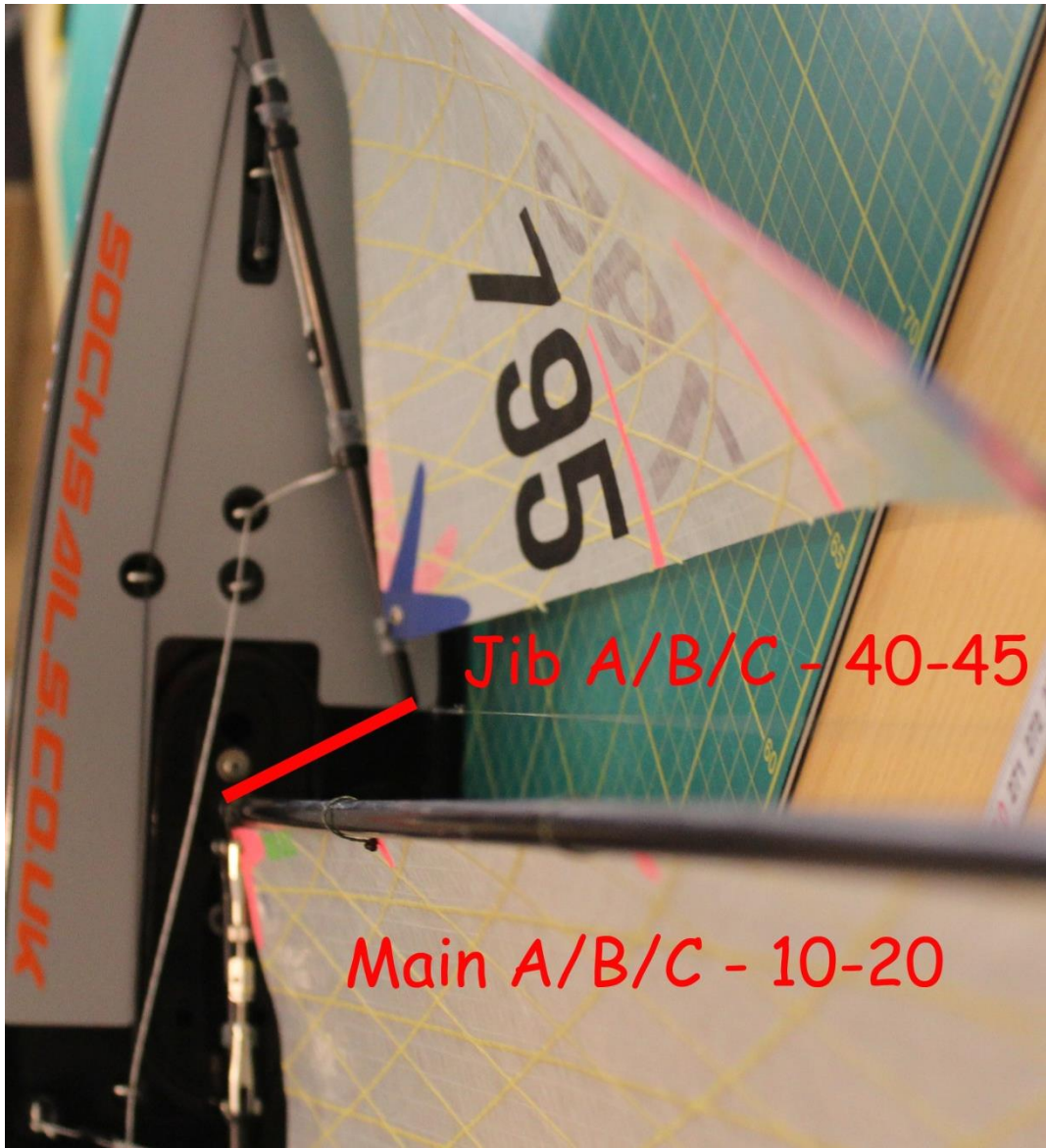
Mast Gate Postion



Fore & Aft Rake



Jib Offset From Centreline



Leach Twist Settings

